

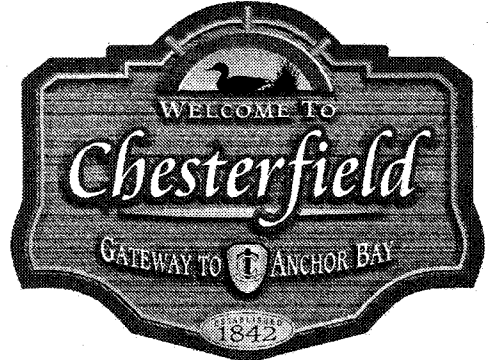


ANDERSON, ECKSTEIN AND WESTRICK, INC.

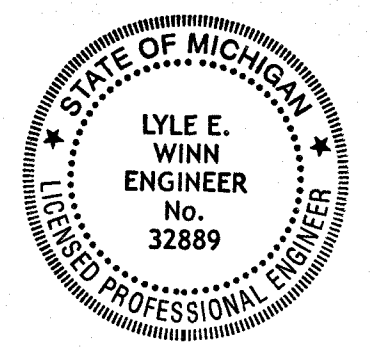
CIVIL ENGINEERS SURVEYORS ARCHITECTS

51301 Schoenherr Road Phone 586 726 1234
Shelby Township Fax 586 726 8780
Michigan 48315

www.aewinc.com
ENGINEERING STRONG COMMUNITIES



11-30-18	TOWNSHIP REVISIONS
DATE	SUBMITTALS/REVISIONS
DRAWN BY: MJS	CHECKED BY: AAP
DATE: SEPTEMBER 2010	DATE:
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Lyle E. Winn
P.E. No. 32889

DEPARTMENT OF PUBLIC WORKS

APPROVED BY: *[Signature]*
BON CODDINGTON, DIRECTOR

DATE: 12/6/18

CHARTER TOWNSHIP OF CHESTERFIELD

STANDARD PAVING DETAILS (2 OF 2)

SHEET NO. PAV2

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GENERAL NOTES

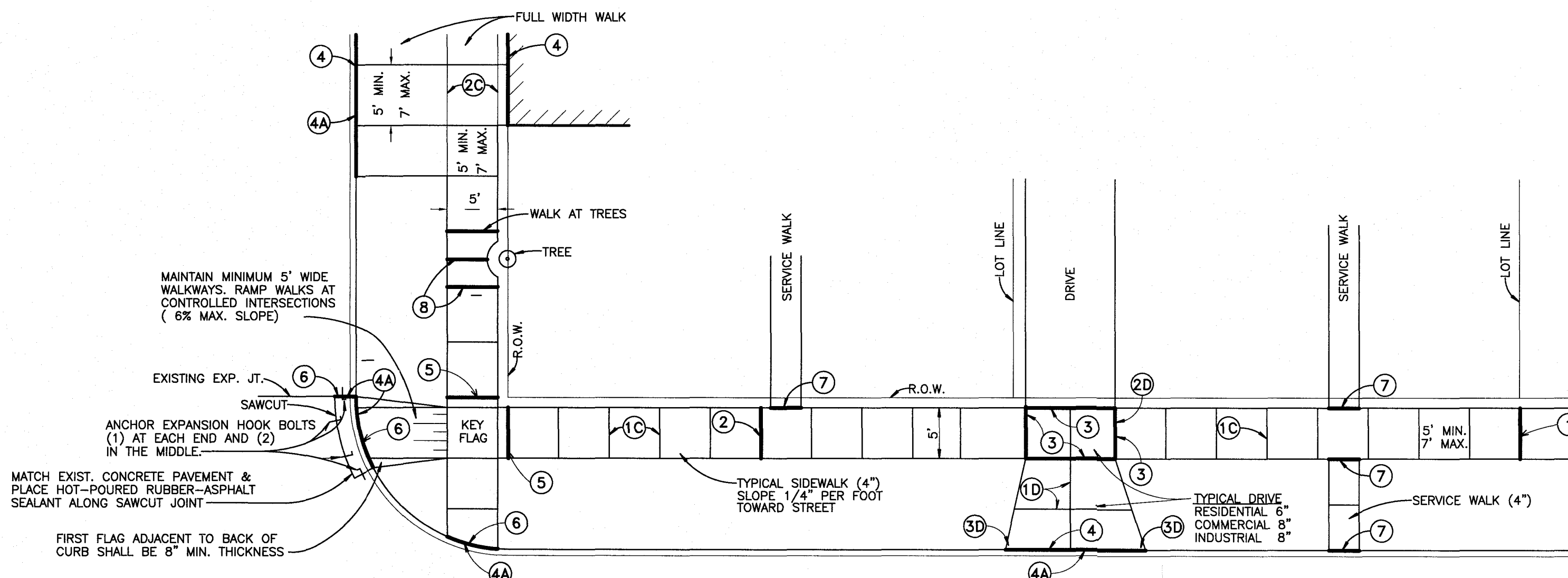
1. PRIOR TO PLACEMENT OF BASE MATERIAL OR PAVEMENTS, THE CONTRACTOR SHALL PROOFROLL THE SUBGRADE IN THE PRESENCE OF A GEOTECHNICAL ENGINEER AND THE DPW AND REMOVE ANY UNSUITABLE MATERIAL OBSERVED.
2. ALL AGGREGATE BASE MATERIAL SHALL BE COMPACTED TO 95% OF ITS MAXIMUM UNIT WEIGHT. A GEOTECHNICAL ENGINEER SHALL TEST FOR PROPER COMPACTION WITH ONE COPY OF THE TEST RESULTS BEING PROVIDED TO THE DPW AND/OR GOVERNING AGENCY.
3. CONCRETE AND BITUMINOUS TESTING IS REQUIRED FOR ALL PROJECTS WITH A COPY OF THE TEST RESULTS BEING PROVIDED TO THE DPW AND/OR GOVERNING AGENCY.
4. IF IN THE OPINION OF THE INSPECTOR/ENGINEER, FIELD CONDITIONS WARRANT ADDITIONAL TESTING, THE DEVELOPER SHALL ARRANGE FOR AND PAY FOR ALL REQUIRED TESTING.

CONCRETE PAVEMENT NOTES

1. CONCRETE SHALL BE MDOT GRADE P1, WITH PORTLAND CEMENT TYPE IA (AIR-ENRAINED) SIX (6) SACK MIX, STRENGTH OF 3500 psi
2. CONCRETE SHALL HAVE A SLUMP BETWEEN 1 1/2" - 3".
3. CONCRETE SHALL HAVE AN AIR CONTENT OF 6% ±1%.
4. ALL CONCRETE PAVEMENT DRIVEWAYS, CURBS, ETC., SHALL BE SPRAY CURED WITH WHITE MEMBRANE COMPOUND IMMEDIATELY FOLLOWING THE FINISHING OPERATION.
5. TEMPERATURE OF CONCRETE AT TIME OF PLACEMENT SHALL BE BETWEEN 45° F AND 90° F.
6. AIR TEMPERATURE AT POINT OF PLACEMENT, AWAY FROM ARTIFICIAL HEAT, SHALL BE AT LEAST 25° F AND RISING.
7. NO CONCRETE PAVING SHALL BE ALLOWED PRIOR TO MAY FIRST OR AFTER NOVEMBER FIRST. (UNLESS APPROVED BY THE DPW SUPERINTENDENT).

BITUMINOUS PAVEMENT NOTES

1. ALL MATERIALS AND METHODS OF HMA PLACEMENT SHALL FOLLOW CURRENT MDOT STANDARD SPECIFICATION.
2. TEMPERATURE AND SEASONAL REQUIREMENTS FOR PLACING HMA WILL BE ACCORDING TO CURRENT MDOT STANDARD SPECIFICATIONS FOR CONSTRUCTION.
3. PLACEMENT OF HMA OR APPLICATION OF BOND COAT IS PROHIBITED WHEN PRECIPITATION IS IMMINENT OR WHEN MOISTURE ON THE EXISTING SURFACE WILL PREVENT SATISFACTORY CURING.
4. ALL BITUMINOUS MATERIAL SHALL BE COMPACTED TO A DENSITY OF 97% OF THE FIELD CONTROL DENSITY AS DETERMINED BY THE MARSHALL METHOD WITH TEST RESULTS BEING PROVIDED TO THE DPW AND/OR GOVERNING AGENCY.
5. RECYCLED HOT MIX ASPHALT MIXTURE: LIMITS RECYCLED ASPHALT MATERIAL (RAP) TO A MAXIMUM OF 15% BINDER BY WEIGHT OF THE TOTAL BINDER IN THE MIXTURE. ASPHALT SHINGLES AND TIRES ARE NOT ALLOWED.



*** EXPANSION JOINTS**

1. PLACE 1/2" FIBER EXP. JOINT FILLERS AT LOT LINES WHEN LOTS ARE 50' OR LESS.
2. ADDITIONAL 1/2" FIBER EXP. JOINT FILLERS FOR LOTS GREATER THAN 50', NOT TO EXCEED 50' INTERVALS.
3. PLACE 1/2" FIBER EXP. JOINT FILLERS EACH SIDE OF DRIVE.
4. PLACE 1" FIBER EXP. JOINT FILLERS AT CURB AND BUILDING OR R.O.W. LINE.
- 4A. AT CURB LINE, EXPANSION JOINTS SHALL BE SEALED WITH HOT-POURED RUBBER-ASPHALT SEALANT.
5. PLACE 1/2" FIBER EXP. JOINT FILLERS AT KEY FLAG OF CROSS-WALKS.
6. PLACE 1" FIBER EXP. JOINT FILLERS AT INTERSECTIONS OF CURB RADIUS AND IN LINE WITH EXISTING JOINTS.
7. PLACE 1/2" FIBER EXP. JOINT FILLER AT INTERSECTION OF SERVICE WALK & CURB ALSO AT SERVICE WALK & WALK.

8. PLACE 1/2" FIBER EXP. JOINT FILLERS AT EITHER SIDE OF SIDEWALK ABUTTING TREE AND AT CENTERLINE JOINT. ROOT BARRIERS MAY BE REQUIRED.

CONTRACTION JOINTS

- 1C. PLACE CONTRACTION JOINTS AT INTERVALS NOT LESS THAN 5' OR GREATER THAN 7', FULL WIDTH OF WALK. JOINT DEPTH SHALL BE 1/4" THICKNESS OF CONCRETE.
- 2C. PLACE CONTRACTION JOINTS AT WALK LINES FOR FULL WIDTH SIDEWALK.

DRIVEWAYS

- 1D. PLACE CONTRACTION JOINTS SO THAT NO SECTION OF THE CONCRETE DRIVEWAY EXCEEDS A DIMENSION OF 10'x10'.
- 2D. PLACE 1" FIBER EXPANSION FILLER AT ALL SIDES OF A COMMERCIAL DRIVE.
- 3D. WIDTH OF APPROACH AT STREET SHOULD BE MIN. 3' WIDER THAN WIDTH AT SIDEWALK, EACH SIDE.

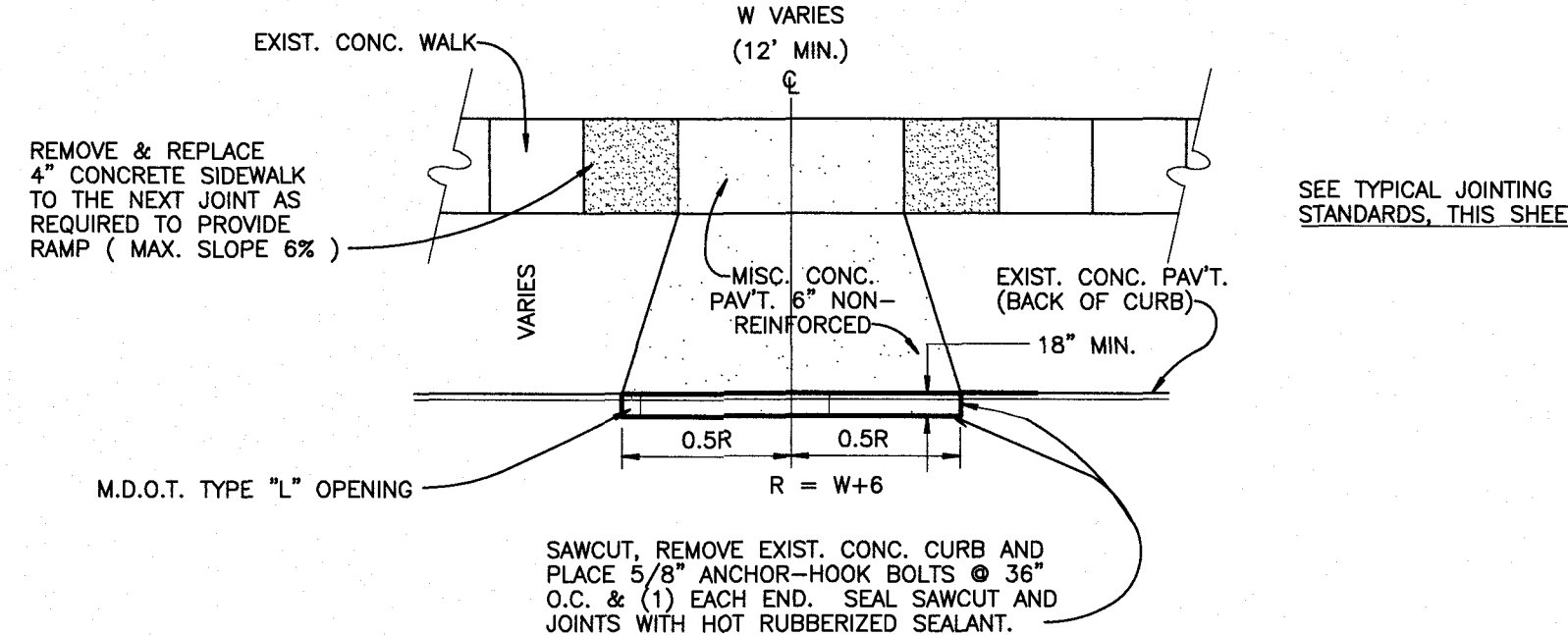
* FIBER EXPANSION JOINT FILLERS SHALL BE FULL DEPTH.

NOTES

1. CONCRETE WORK NOT CONSTRUCTED UNDER THE TOWNSHIP'S CONTRACTOR MUST BE DONE UNDER A PERMIT, SECURED THROUGH THE CHARTER TOWNSHIP OF CHESTERFIELD DEPARTMENT OF PUBLIC WORKS.
2. NO CONCRETE SHALL BE PLACED UNTIL A PRE-CHECK HAS BEEN REQUESTED BY THE CONTRACTOR OR HOME-OWNER AND APPROVED BY THE CHARTER TOWNSHIP OF CHESTERFIELD D.P.W. (949-0400 EXT. 3) - AT LEAST (48) HOURS IN ADVANCE. FINAL INSPECTION IS ALSO REQUIRED AT THE PLACEMENT OF CONCRETE.
3. ALL WORK SHALL CONFORM TO THE CHARTER TOWNSHIP OF CHESTERFIELD STANDARDS & SPECIFICATIONS AVAILABLE THROUGH THE TOWNSHIP ENGINEER.
4. HANDICAP RAMPS SHALL MEET A.D.A. BARRIER FREE REQUIREMENTS.
5. TRUNCATED DOME DETECTABLE WARNING PLATES SHALL BE CAST IRON - BLACK IN COLOR

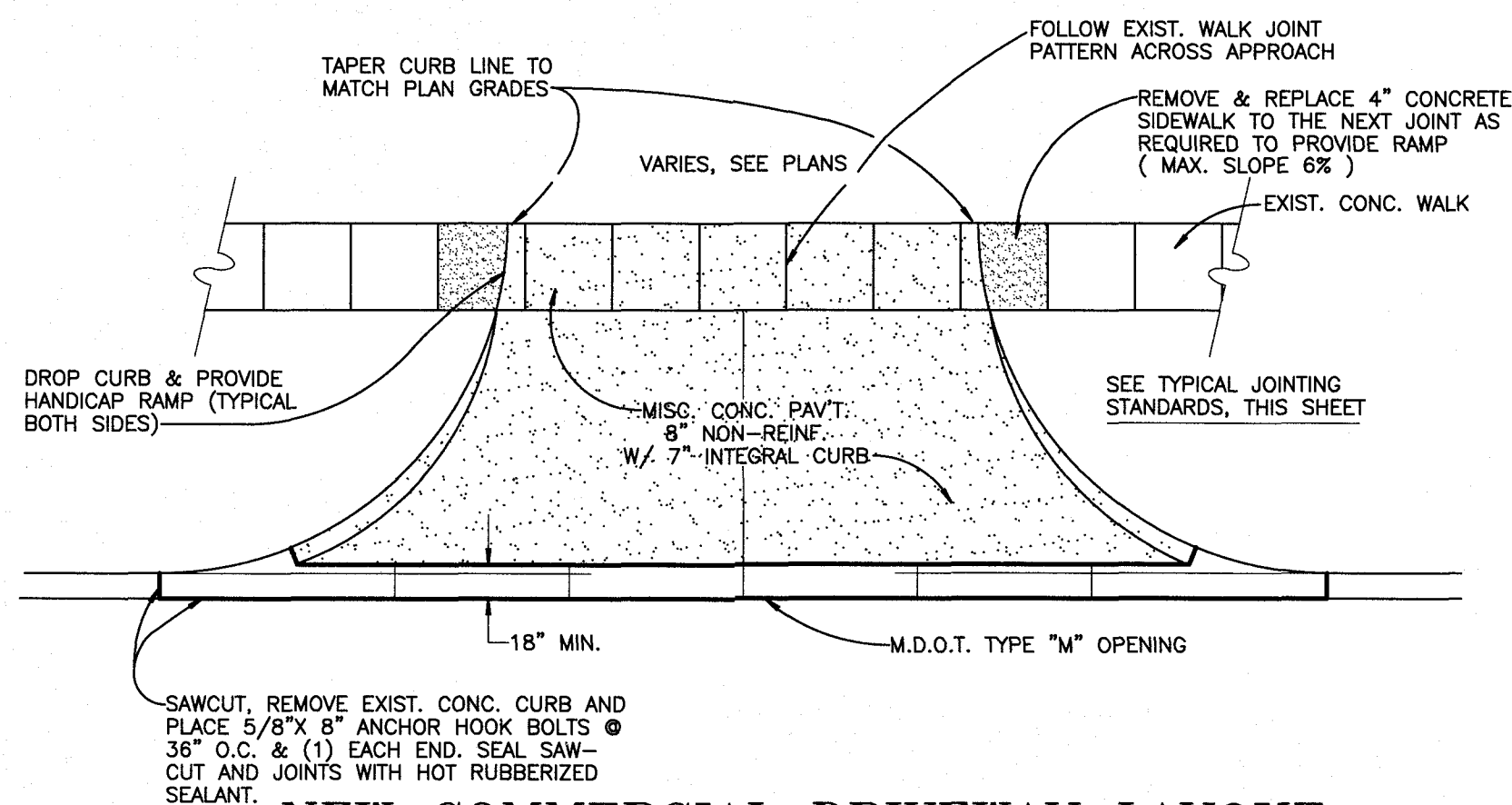
TYPICAL SIDEWALK & DRIVE APPROACH JOINTING STANDARDS

NO SCALE



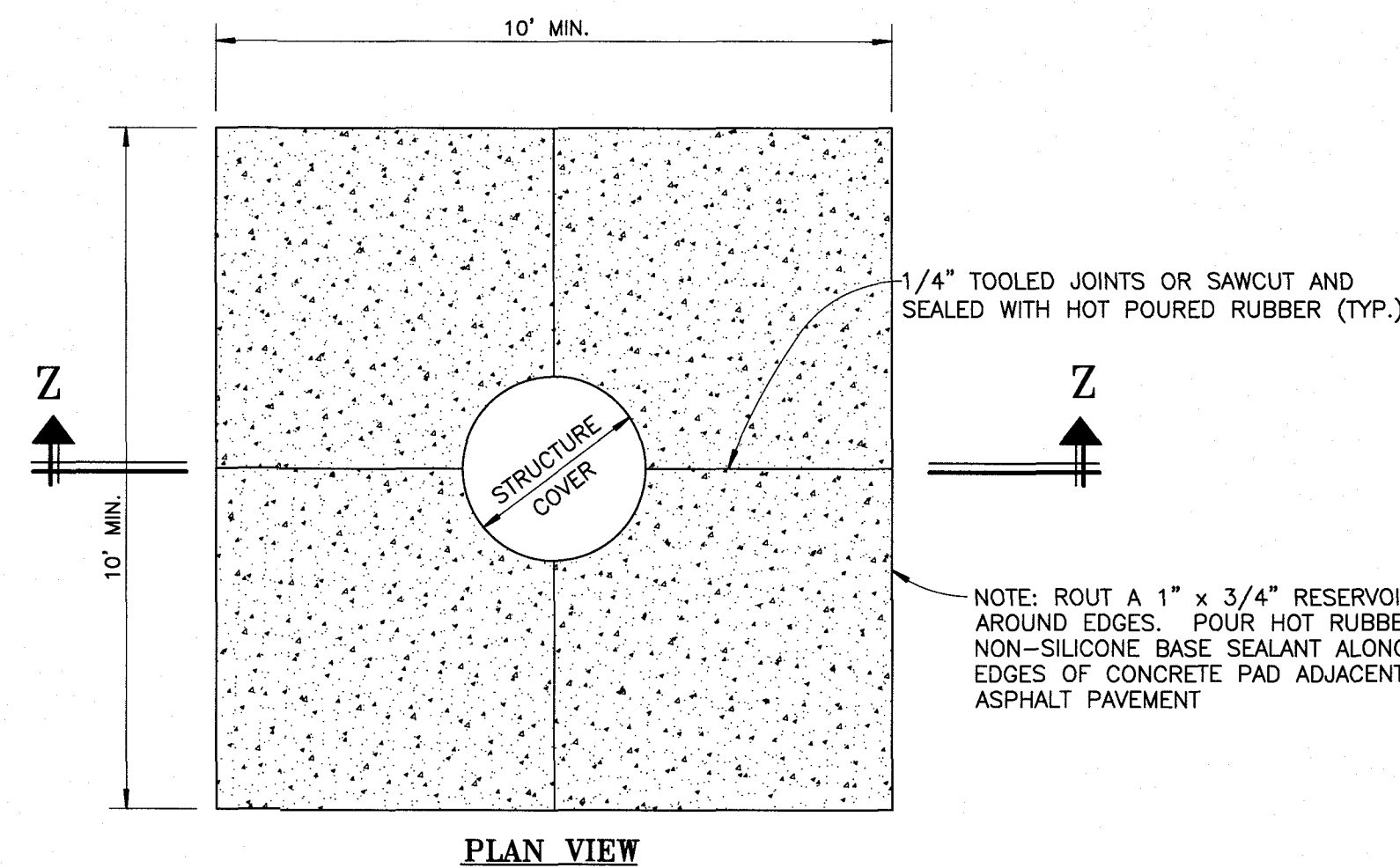
NEW RESIDENTIAL DRIVEWAY LAYOUT FOR EXISTING PAVEMENT CONDITION

NO SCALE

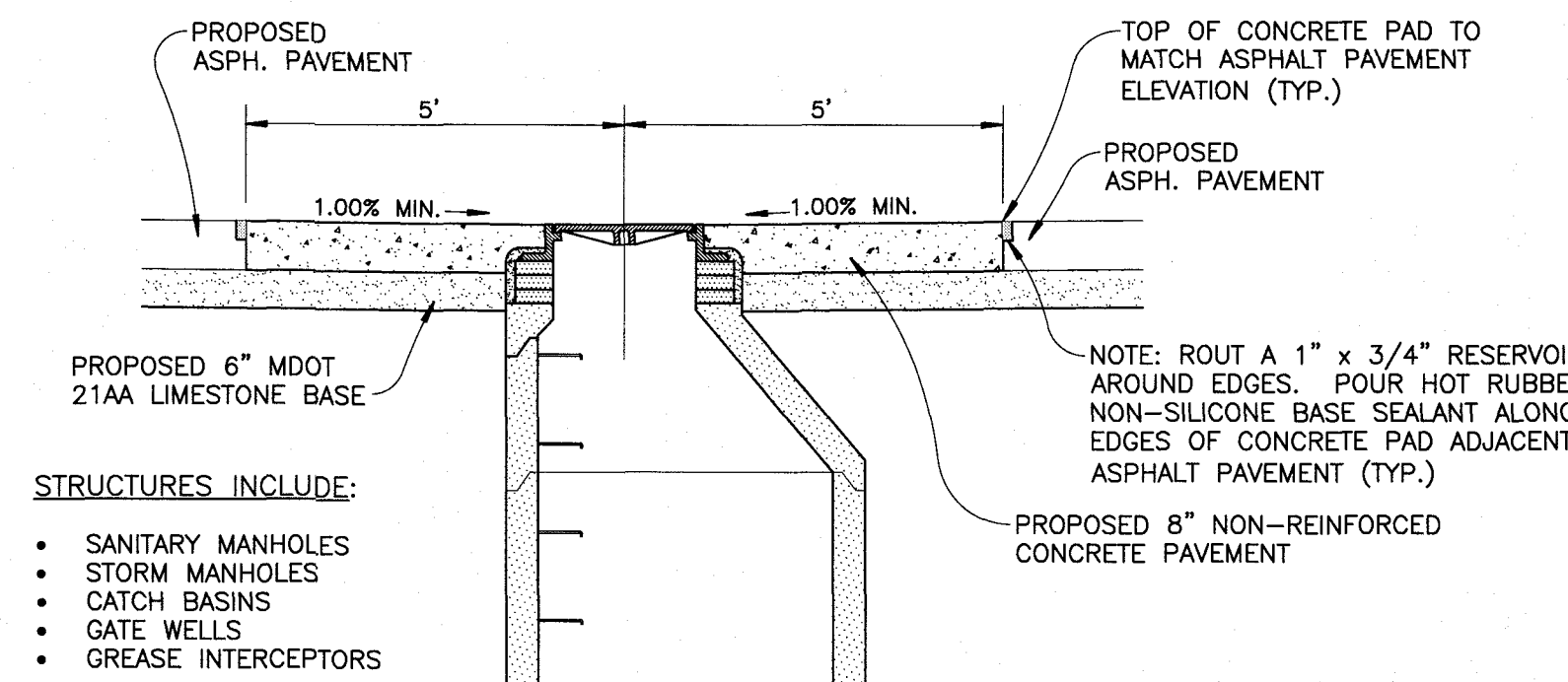


NEW COMMERCIAL DRIVEWAY LAYOUT FOR EXISTING PAVEMENT CONDITION

NO SCALE



PLAN VIEW

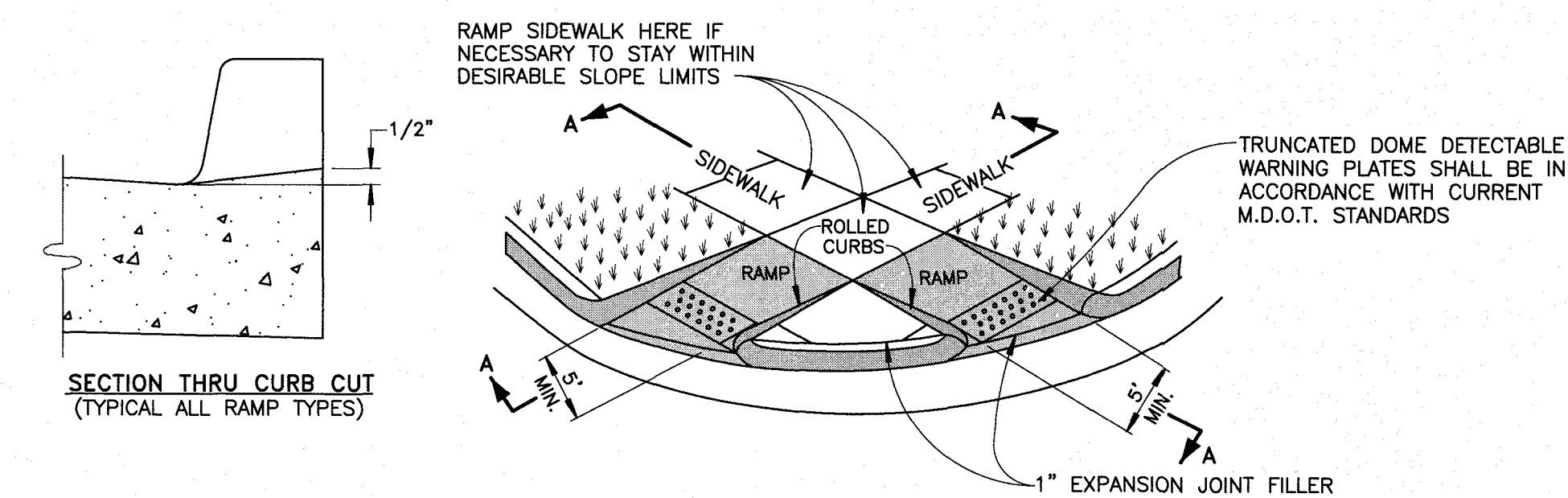


SECTION Z-Z

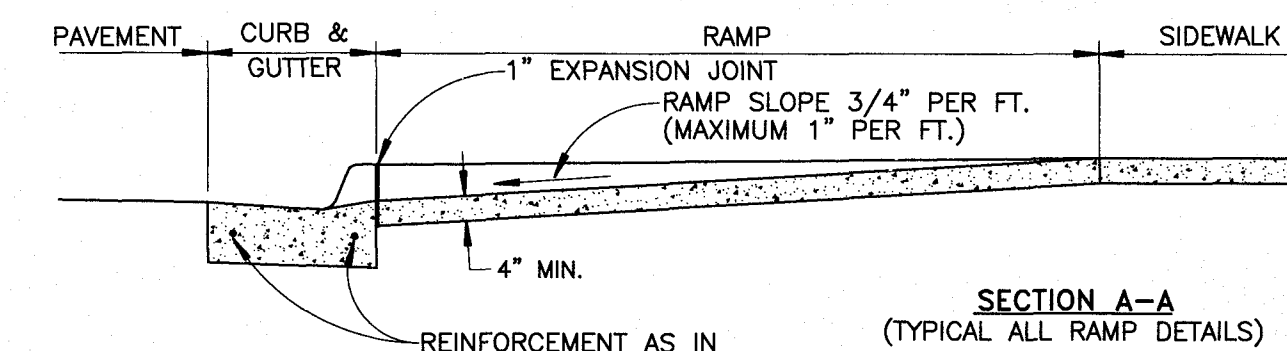
CONCRETE PAD DETAIL AT STRUCTURE IN ASPHALT PAVEMENT

NOT TO SCALE

- STRUCTURES INCLUDE:
- SANITARY MANHOLES
 - STORM MANHOLES
 - CATCH BASINS
 - GATE WELLS
 - GREASE INTERCEPTORS



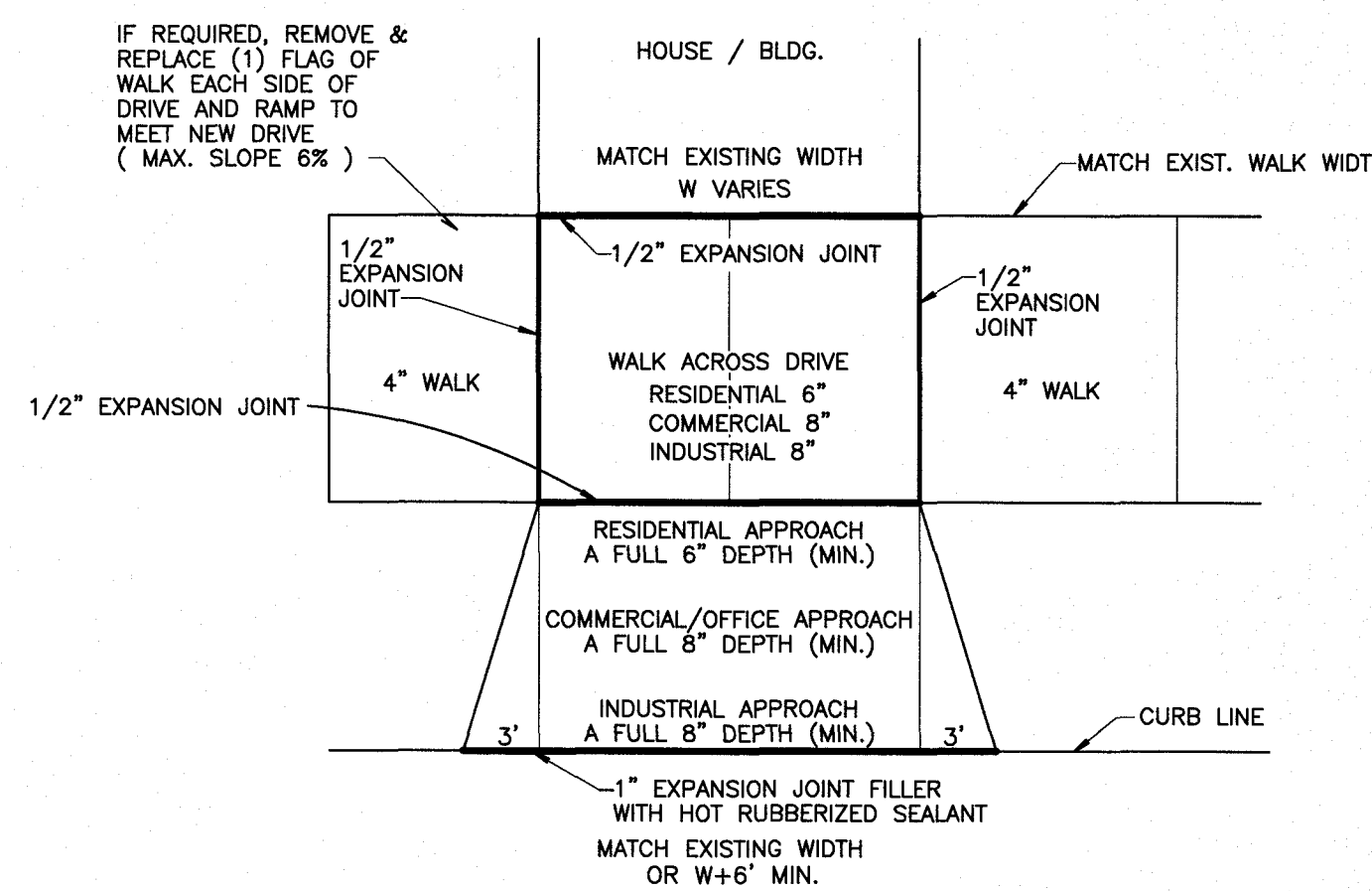
SECTION THRU CURB CUT (TYPICAL ALL RAMP TYPES)



SECTION A-A (TYPICAL ALL RAMP DETAILS)

TYPICAL RAMP INSTALLATION

NOT TO SCALE (M.D.O.T. TYPE 4)



REMOVAL AND REPLACEMENT OF EXISTING DRIVE APPROACH FOR EXISTING OR NEW PAVEMENT CONDITION

NO SCALE